

11th CSPCWG-1st NCWG MEETING
Rostock, Germany 27-30 April 2015

Paper for Consideration by NCWG
Refuge Anchorage/Area

Submitted by:	UK
Executive Summary:	Italy established a 'refuge area' in Port of Augusta – how should this be charted?
Related Documents:	S-4; INT1
Related Projects:	None

Introduction / Background.

Italian NM:

- 16.27 - 6-VIII-2014
Augusta – Port information

Insert after line 39:

“8) Procedures for accommodating vessels needing assistance in the refuge area of the port of Augusta (Ordinance no. 5/2014) – In the case of vessels requiring assistance, a “refuge area” has been identified inside Porto Megarese, within the Augusta port area. It is put into effect when circumstances so require and is delimited by the following positions (WGS84):

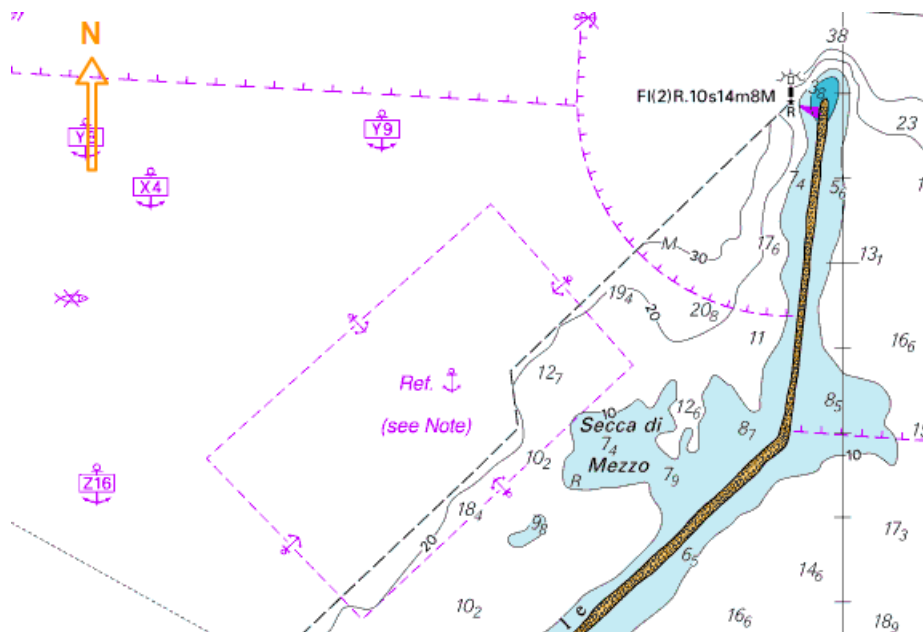
- a) 37°11'.27 N – 15°13'.06 E
- b) 37°11'.57 N – 15°13'.48 E
- c) 37°11'.08 N – 15°13'.29 E
- d) 37°11'.38 N – 15°13'.69 E.

Depth measurements in the “refuge area” vary from more than 20 m in the N part, down to 7 m in the part near the outer dyke. The seabed is muddy.”.

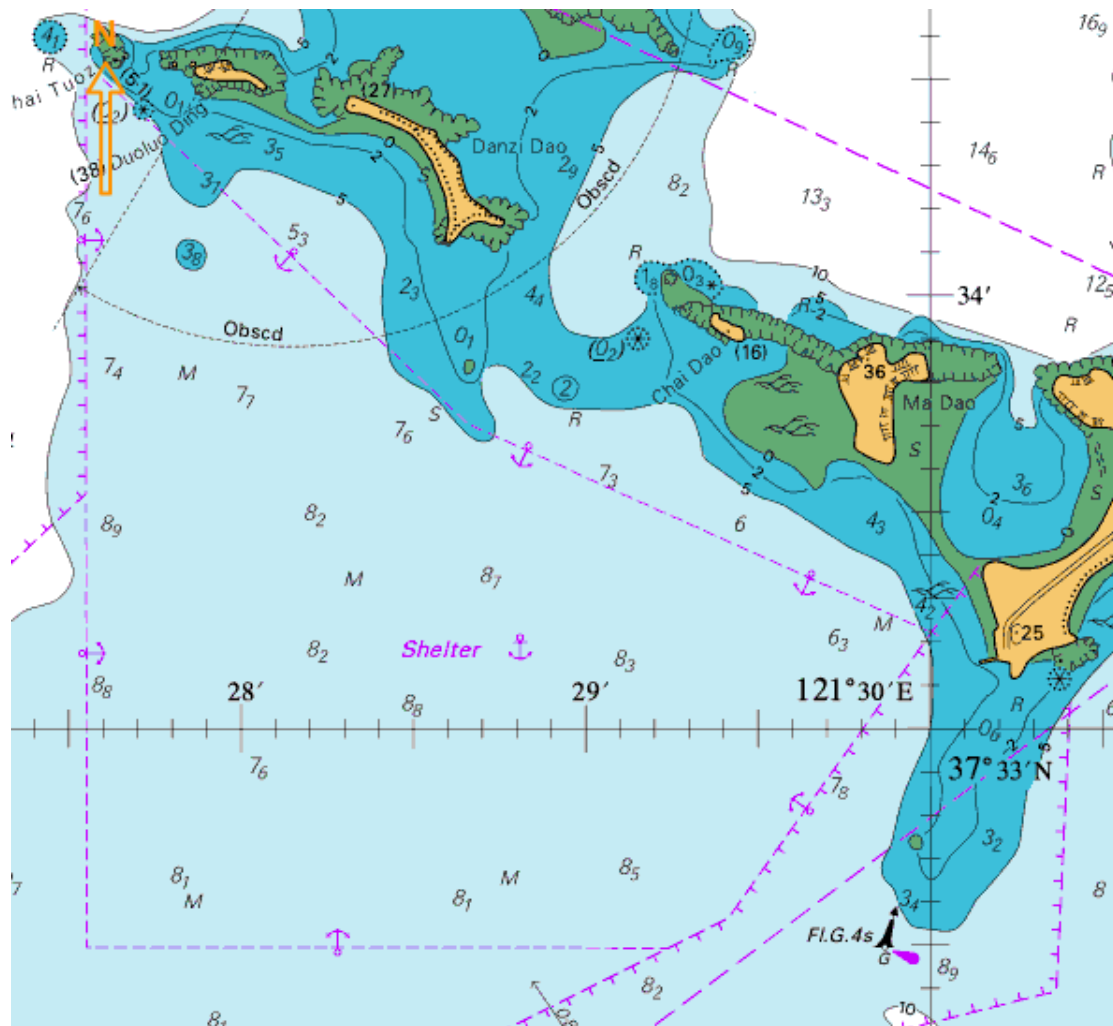
Sailing Directions P5, 2014 edition, page 145

(File 624/2014)

UKHO chose to include this on the relevant chart as:



Another area, possibly having the same purpose, was derived from a Chinese chart, using the English legend on that chart:



Analysis / Discussion.

In the case of the Italian NM, the word 'refuge' is used. An assumption is made that the area is an anchorage (because of the reference to the muddy seabed), so the area has been shown as an anchorage using N12.1-9 adapted as a Refuge anchorage by using the international abbreviation 'Ref' alongside the anchor (the addition of the full stop is an error).

If the area was simply a refuge area, rather than an anchorage, then the limit would be N1.2, with a sloping, magenta 'Ref' inside (possibly in full as 'Refuge Area' if the extent of the area is large).

It would probably have been better to use the international abbreviation for the Chinese anchorage, rather than the English word 'Shelter', which is really a synonym for refuge.

At present, the international abbreviation 'Ref' only appears in black upright text in S-4 (and INT1). References are: B-120 (INT abbreviation), B-370.8 (Buildings, especially in Antarctica), B-456.4b (Beacon Q124), B-493.3 (Refuge in general T14). Therefore the use of refuge associated with an anchorage or maritime area is an extension of the use of the international abbreviation.

Although the fact that 'Ref' is listed in S-4 B-120 and in INT1 as an international abbreviation for 'Refuge' allows for its use in contexts additional to the INT1

references Q124 and T14, it is suggested that this could be mentioned in S-4:

at B-431.3 (changes in red):

Numbered or named anchorage areas, or anchorages for particular vessels, should be identified as in the following examples (sloping text, anchor symbol upright, all magenta) where possible. These symbols may be adapted for other types of vessels **or purpose**, for example small craft, **refuge area** (*Ref*). Size of text and associated anchor symbol may be adjusted to suit the size of the area.

and at B-493.4:

A refuge for ~~shipwrecked~~ vessels and/or mariners should be charted by the **international abbreviation**

Ref or *Ref* **T14**

The abbreviation should be inserted adjacent to an appropriate symbol, such as a building (D5) or a refuge beacon (Q124), **or within a maritime area such as an anchorage (N12)**.

This application is covered in INT1 in the footnote to N12: 'Other types of anchorage area may be shown'.

Perhaps the word 'shipwrecked' should be removed, as it may not always be the case that mariners requiring refuge are actually shipwrecked. A magenta sloping version *Ref* could be added to INT1 T14 and the description would also need to change to be consistent with S-4.

Conclusions

Refuge areas and/or anchorages should be indicated by the existing agreed international abbreviation 'Ref', adapted for association with a maritime area (therefore magenta, sloping text).

Recommendations.

Small changes (clarifications) to S-4 B-431.3 and B-493.4 would accommodate this. A small change to INT1 T14 is also recommended.

Justification and Impacts.

For consistent application of the term 'Refuge' on charts.

Action required of NCWG.

The NCWG is invited to endorse the above conclusion and recommendation for two clarifications to S-4 and a minor amendment to INT1 T14.