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Danish trials of virtual AIS

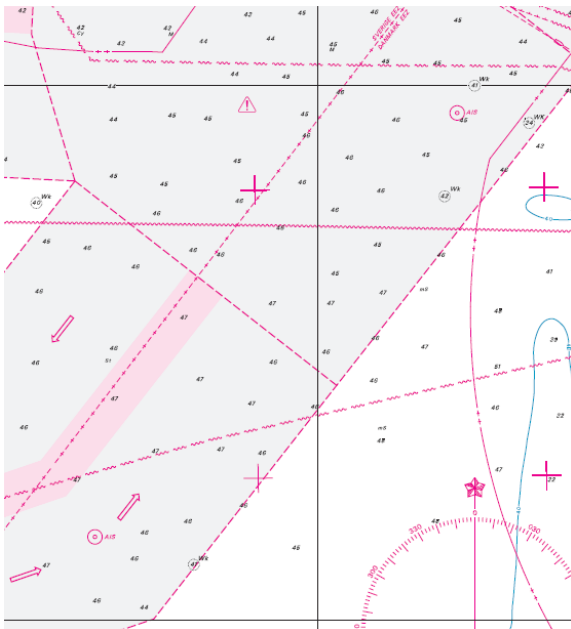
Submitted by:	Denmark
Executive Summary:	To give a brief account on the Danish trials of virtual AIS
Related Documents:	-
Related Projects:	-

Introduction

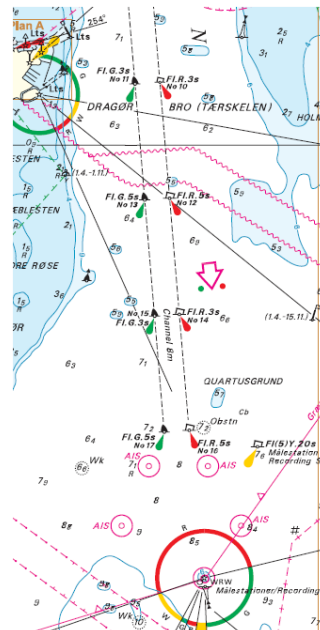
Since 2008 the Danish Maritime Safety Administration has used virtual AIS as a supplement to physical AtoN in Danish waters.

Discussion

In June 2008 a number of virtual AIS were established for a test period. Two as waypoints in TSS Bornholmsgat as an aid to the eastbound traffic and four as lateral buoys in Drogden dredged channel to make the southern approach more distinct.



DK Chart 189 Bornholmsgat



DK Chart 133 Sundet, Midterste del

In this test period the users were through a note published in NtM asked to give feedback and both the pilots and the Administration's own vessels joined in the analysis. The 20 reports from the pilots revealed that only in one case the virtual AIS were actually visible on the ECDIS.

Since then the Maritime Safety Administration has evaluated on the experiences and realized the importance of testing new technologies and the need of international standards.

As a result of the experiences there have been no new virtual AIS in Danish waters except some virtual AIS markers on the Danish production platforms in the North Sea. They are listed as virtual AIS in NtM but are in fact markers on the individual constructions.

The future

When the standards as to how virtual AIS should be displayed are implemented The Danish Maritime Safety Administration believes that the use of virtual AIS will be more common. Virtual AIS will never completely

replace physical buoys but will be used as a supplement. To the Administration's point of view it is of vital importance to have physical buoyage as a backup in case of digital malfunctions or power failure.

The Administration hopes that the use of class B AIS transponder in pleasure crafts will develop accordingly. Today it is not mandatory for the pleasure crafts to carry AIS transponder, but in the future there might be a greater need to distinguish all types of vessels to improve the navigational safety at sea.

Conclusion

Both the Danish Maritime Authority and we, as being the national chart producer, disapprove of the Administration's solitary approach to this whole matter. From our point of view both we as well as our neighbouring countries should have been consulted before implementing virtual AIS in Danish waters.

There is still no consensus on the matter in DK even though the Maritime Safety Administration keeps publishing new virtual AIS in NtM.